

# Residency at Empire II

## 337 Units 100% Affordable Housing Development with One incentive.

### 3000 W. Empire Ave, Burbank CA 91504

## PROJECT INFORMATION

### ADDRESS/LEGAL INFORMATION

ADDRESS: 3000 W. EMPIRE AVE, CITY OF BURBANK, CA 91504  
 APN: 2464-001-019  
 LEGAL DESCRIPTION:

THE LANO REFERRED TO HEREIN BELOW IS SITUATED IN THE COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AND IS DESCRIBED AS FOLLOWS:

PARCEL 1 IN THE CITY OF BURBANK, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS PER MAP RECORDED IN BOOK 63, PAGE 44 OF PARCEL MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY.

APN(S): 2464-001-019

GROSS LOT AREA: 85,922 S.F +-  
1.973 AC +-

### JURISDICTIONAL INFORMATION

BURBANK MUNICIPAL CODE TITLE 10 ZONING REGULATIONS  
 BURBANK 2035 GENERAL PLAN

### PLANNING AND ZONING INFORMATION

ZONING:	M-2 (General Industrial)	Incentives/Concessions	Code Section
General Plan Designation	Regional Commercial Consistent with the General Plan	None required	
Allowable FAR	58 units/acre	No density restrictions per Government Code	Government Code Section 65915(f)(3)(D)(ii).
Allowable Density	58 units/acre	No density restrictions per Government Code	Government Code Section 65915(d)(2)(D)
Allowable Height	50 ft	77'-4.5" proposed allowed additional 3 stories or 33 ft project within 1500 ft major bus stop. per Government Code	Government Code Sections 65915(d)(2)(D)
Required Front yard (20% bldg ht=77.38x.0.20)	15 ft -6 inches	0 feet proposed incentive #1	Government code Section 65915(d)(2)(D)
Open Space	Not Required		
Base Parking Requirement:		Zero parking required project within 1500 ft major bus stop.	Government Code Sections 65915(p)(3)
A. ZERO TO ONE (1) BEDROOMS: ONE (1) ONSITE PARKING			
B. TWO (2) TO THREE (3) BEDROOMS: TWO (2) ONSITE PARKING			
C. FOUR (4) AND MORE BEDROOMS: TWO AND ONE-HALF (2 1/2)			

### PROJECT SUMMARY:

A 347 UNIT- 7 STORY- TYPE 1A & TYPE 111A -ONE HOUR-FULLY NFPA 13 SPRINKLERED DENSITY BONUS -100 % AFFORDABLE WITH NO REQUIRED PARKING SB35 Ministerial approval

BUILDING DESCRIPTION  
 337 UNIT 7 STORY APARTMENT BUILDING  
 CONSISTING OF 5 STORIES TYPE 111A APARTMENT  
 OVER TWO STORIES TYPE 1A APARTMENT AND GARAGE  
 3 HOUR FIRE WALLS ARE USED FOR AREA LIMITATIONS.

### AREA BY OCCUPANCY

R2 - Residential 272,944 s.f  
 S1 - Parking garage 28,556 s.f.  
 Total Building Area 301,500 s.f.

Vertical shaft wall:  
 2 hour with 90 minutes door assemblies  
 Fire Walls  
 3 hour non-combustible with 3hr door assemblies

### BUILDING CODE INFORMATION:

CONSTRUCTION TYPE: 5-TYPE 111A over 2-TYPE 1A  
 OCCUPANCY: R2 / S2  
 NO. OF STORIES: 7  
 TOP OF PARAPET ELEV. 733.84 (excluding equip screen)  
 AVERAGE GRADE PLANE: 656.46  
 ZONING CODE HEIGHT: 77'-4.5"  
 BUILDING CODE HEIGHT: 78'-6" complies with Tbl 504.3 measured to top of roof  
 FULLY SPRINKLERED NFPA 13

### FIRE RESISTIVE RATING FOR TYPE 111A CONST:

PRIMARY STRUCT FRAME: 1 HR  
 BEARING EXTERIOR WALLS 2 HR  
 BEARING INTERIOR WALLS 1 HR  
 NON BEARING INTERIOR 1 HR  
 NON BEARING EXTERIOR 1 HR  
 FLOOR CONSTRUCTION 1 HR  
 ROOF CONSTRUCTION 1 HR

Note: All exterior walls must be constructed with Fire Retardant Treated Wood for Type 111 CONSTRUCTION TYPE 111A construction throughout

### GOVERNING CODES:

1. All projects shall comply with Title 9, Chapter 1, of the Burbank Municipal Code, 2019 edition of the California Building Code, California Residential Code, California Electrical Code, California Mechanical Code, California Plumbing Code, California Green Building Standards and Building Energy Efficiency Standards, including all intervening Code cycles.

### FIRE DEPARTMENT REQUIREMENTS

PROVIDE AUTOMATIC SPRINKLER SYSTEM THROUGHOUT ENTIRE BUILDING. NFPA -13  
 OBTAIN PLUMBING PERMIT PRIOR TO INSTALLATION.  
 PROVIDE MANUAL FIRE ALARM SYSTEM PER CBC 907  
 PROVIDE 2-WAY COMMUNICATION SYSTEM PER LAFD 510  
 BUILDINGS SHALL BE PROVIDED WITH APPROVED RADIO COVERAGE FOR EMERGENCY RESPONDERS LAFD 510  
 PROVIDE ELEVATOR BATTERY LOWERING + LIGHTING  
 PROVIDE 2 HR EMERGENCY BATTERY BACK-UP POWER SUPPLY.  
 PROVIDE DISTRIBUTED ANTENNA SYSTEM (DAS) PER NFPA & FD REQUIREMENTS

BUILDING CODE AREA ANALYSIS CBC Tbl: 506.2 for Type 111A construction.  
 A<sub>1</sub> = 24000 X2 = 48,000 s.f  
 Proposed R2 floor area above concrete podium = 204,500 s.f  
 3 hour Fire wall used to reduce fire area within the structure  
 DEXTOX DECK COATING ESR 1757  
 ROOF - DURALAST  
 A COPY OF THE EVALUATION REPORT AND/ OR CONDITIONS OF LISTING SHALL BE MADE AVAILABLE AT THE JOB SITE.

### FIRE RESISTIVE RATING FOR TYPE 1A CONST:

PRIMARY STRUCT FRAME: 3 HR  
 BEARING EXTERIOR WALLS 3 HR  
 BEARING INTERIOR WALLS 3 HR  
 NON BEARING INTERIOR 0 HR  
 NON BEARING EXTERIOR 1 HR  
 FLOOR CONSTRUCTION 2 HR  
 ROOF CONSTRUCTION 1 1/2 HR

## UNIT MIX

FLOOR	STUDIO	1 BRM	2 BRM	3 BRM	TOTAL
1	1	11	8	7	27
2		26	11	13	50
3		26	13	13	52
4		26	13	13	52
5		26	13	13	52
6		26	13	13	52
7		26	13	13	52
TOTAL	1	167	84	85	337

## Parking Analysis

Parking Required	none	4 Incentives Allowed
Parking Provided:		1 Incentives Request
Standard Stalls	35	
Van Accessible + 1	2	
EVCS van +1 accessible	2	
EV	15	
Total Provided	54	
Bicycle Parking Analysis		
Parking Required	none	
Parking Provided:		
Longterm Stalls	110	
Total Bicycle Parking	110	

1) 0' front yard instead of 15'-6"

## Development Standards

- Landscaping.
  - A minimum of 50 percent of the exposed side yards shall be landscaped.
  - The provision of outdoor amenities and decorative hardscape, such as outdoor seating areas with benches permanently affixed to the ground or landscaped areas enriched with decorative materials which are under a tree canopy, shall be credited toward up to 50 percent of the required landscaping in all yards. Vehicular access areas may not be considered as decorative hardscape.
  - The planting of vines on masonry buildings is encouraged.
  - To qualify as landscaped area, all areas not occupied by trees or shrubs must be planted with turf or other ground cover with a minimum soil depth of 12 inches. All planters must be a minimum of 18 inches deep and two feet in their smallest inside dimension, unless a tree is required, in which case a three (3) foot planter depth shall be required and the planter must have a minimum inside dimension of four (4) feet.
  - In required exposed side yards, a minimum of one tree shall be planted for every 40 linear feet of street frontage or fraction thereof. Turf is allowed in up to 50 percent of required landscaped areas. In shrub areas, a minimum of one five (5) gallon shrub is required for every ten (10) square feet of shrub area. There is no front yard per SB 1818. Side yard Landscape compliance met.
  - A minimum of 50 percent of required trees shall be a minimum 36-inch box size, with the remainder a minimum 24-inch box size. The required 36-inch box trees shall be equally distributed in required front or street side yards.
  - If trees are planted in planters, the planters must have a minimum length and width of five (5) feet.

## PROJECT TEAM

OWNER/DEVELOPER ABS PROPERTIES, INC. 5500 HOLLYWOOD BLVD 4TH FLOOR, WEST WING LOS ANGELES, CA 90028	CIVIL ENGINEER LAND DESIGN CONSULTANTS, INC 800 ROYAL OAKS DR, SUITE 104 MONROVIA, CA 91016 626-578-7000	MECHANICAL ENGINEER M. WEBER ASSOCIATES, INC 15501 SAN FERNANDO MISSION BLVD #310 MISSION HILLS, CA 91345 (818) 838-9102 - med4med@mwaengineers.com	TRAFFIC ENGINEER George Rhyner, PE KOA 300 Corporate Pointe, Suite 470, Culver City, CA 90230 T (310) 473-6508   D (310) 961-4366   C (310) 779-8051 www.koacorp.com
ARCHITECT EDWIN MOHABIR ARCHITECT, INC 25206 BISHOP CT VALENCIA, CA 91381 323-459-8809	LAND USE INCARMBRUSTER, GOLDSMITH & DEVAC LLP 11611 SAN VICENTE BLVD, SUITE 900 LOS ANGELES, CA 90049 310-209-8800	PLUMBING ENGINEER M. WEBER ASSOCIATES, INC 15501 SAN FERNANDO MISSION BLVD #310 MISSION HILLS, CA 91345 (818) 838-9102 - med4med@mwaengineers.com	ACOUSTICAL ENGINEER Jeffrey K. Pack EDWARD L. PACK ASSOCIATES, INC. 1975 Hamilton Ave. Suite 26 San Jose, CA 95124 Office: (408) 371-1195 Cell: (408) 921-4886
STRUCTURAL ENGINEER LABIB FUNK-ASSOC. 319 MAIN ST. EL SEGUNDO, CA 90245 213-239-9700	LANDSCAPE ARCHITECT: DSK Landscape Architects, INC 3000 W. 6th Street, Suite 300 Los Angeles, CA 90020 Office: 213-382-3113 Cell: 310-259-8650	ELECTRICAL ENGINEER LAZ DESIGN ENGINEERING 1314 W. GLENDALE BLVD #205 GLENDALE CA 91202 (818)937-9229 lazdesignsus.onmicosoft.com	

## UNIT MATRIX-MOBILITY FEATURES UNIT PER 2021 TCAC

Min. Percent of Mobility units = 15% of all units  
 148 units x 0.15 = 12.2 therefore total 23 units with Mobility features units are required

Unit Types	Total No. of Unit	Mobility Features Units Required	Provided	Unit Location
STUDIO	1	1	1	
1 BRM	167	25	25	
2-Bedroom	84	13	13	
3-Bedroom	85	13	13	
Total	337	52	52	52 MOBILITY UNIT PROVIDED



## UNIT MATRIX-COMMUNICATION FEATURES UNIT PER 2021 TCAC

Min. Percent of Communication Features Units Required: 10% of all units  
 148 units x 0.1 = 14.8 therefore total 15 Communication features units are required



Unit Types	Total No. of Unit	Communication Features Units Required	Provided	Unit Location
1 BRM	168	16.8	17	
2-Bedroom	84	8.4	9	
3-Bedroom	85	8.5	9	
Total	337	33.7	34	34 COMMUNICATION UNITS PROVIDED

\*This is a publicly funded housing project and must comply with federal accessibility standards and California Building Code Chapters 11A & 11B.

Source of Public Funding:  
 FY 2021 FEDERAL TAX CREDITS  
 FY 2021 CA STATE TAX CREDITS  
 FY 2021 TAX EXEMPT BONDS

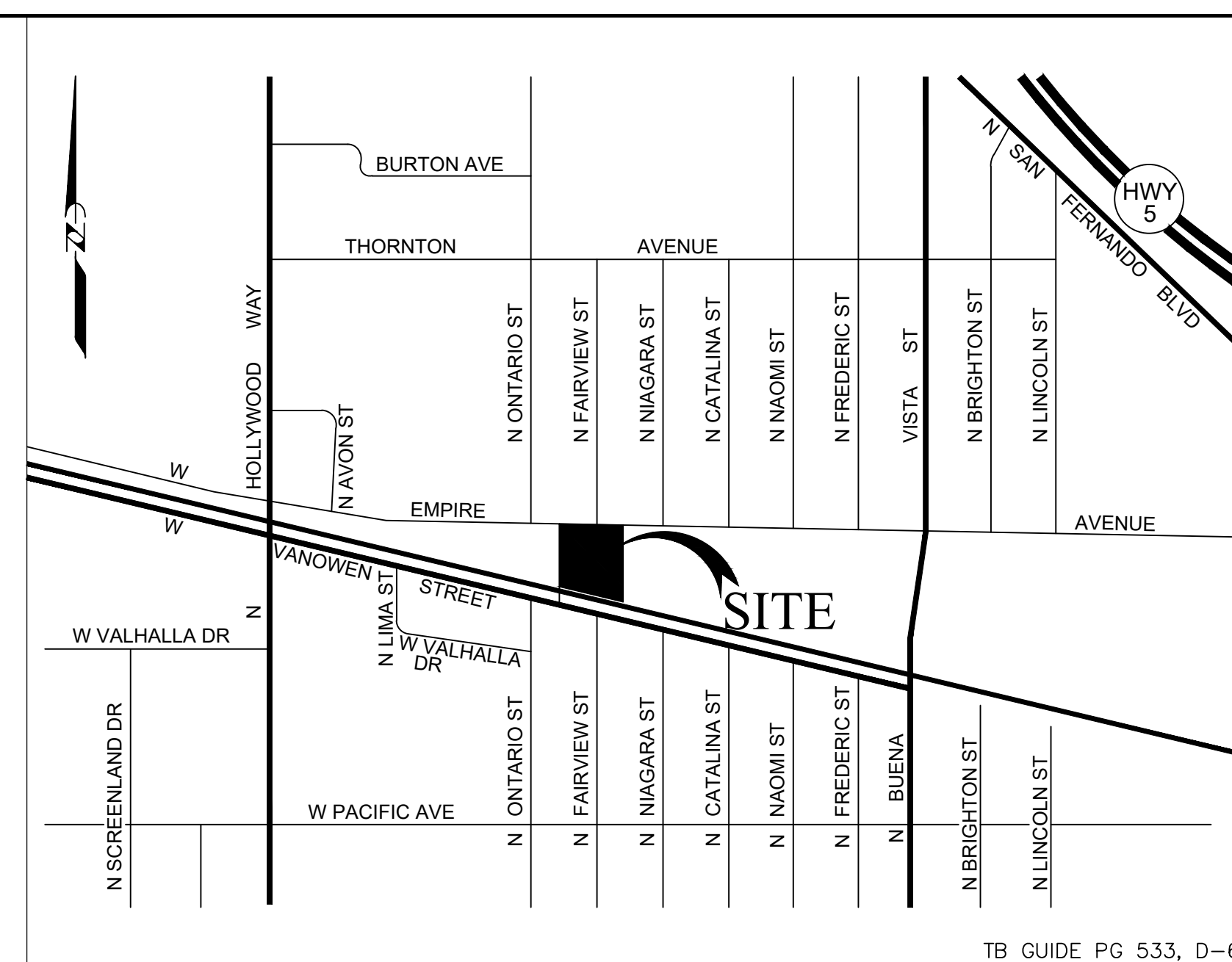
- Applicable Codes
- 2021 California Tax Credit Allocation Committee Regulation Section 10325(f)(7)(K)
  - 2021 California Tax Credit Allocation Committee Regulation Section 10325(g)(2)(B)
  - 2019 California Building Code Chapter 11A & 11B
  - 2010 ADA TITLE II, 2010 ADA Standards for Accessible Design
  - Universal Design is not used in this project for TCAC points

Deferral of any submittal items shall have prior approval of building official. The registered design professional in responsible charge shall list the deferred submittals on construction documents for review.

SEPARATE PERMIT APPLICATION FOR: GRADING WORK R&R, BLOCK FENCE WALLS, SIGNS, FIRE SPRINKLER, FIRE ALARM SYSTEMS, ELECTRICAL, MECHANICAL, PLUMBING WORK, AND DEMOLITION

## VICINITY MAP

## BUS STOP DISTANCE



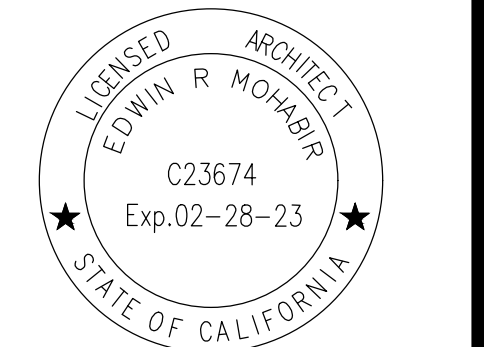
VICINITY MAP  
 NOT TO SCALE

EDWIN MOHABIR

# EM

ARCHITECT, INC.

25206 BISHOP CT.  
 STEVENSON RANCH, CA 91381  
 EDWINMOHABIR@GMAIL.COM  
 TEL: 323-459-8809



PROJECT ADDRESS:  
 337 AFFORDABLE  
 HOUSING DEVELOPMENT

### Residency at Empire II

3000 W. EMPIRE AVE  
 BURBANK  
 CA 91504

DEVELOPER INFORMATION:

**ABS Properties, Inc.**  
 5500 HOLLYWOOD BLVD., 4TH FLR  
 LOS ANGELES, CA 90028  
 (213)268-2723

NOTES/REVISIONS:

PROJECT DATA:

DATE: 12-28-22  
 ARCHITECT: EM  
 CHECKED BY: EM  
 DRAWN BY: EM  
 PROJECT NO.:

SCALE: 1/8" = 1'-0"

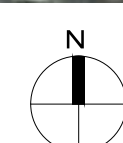
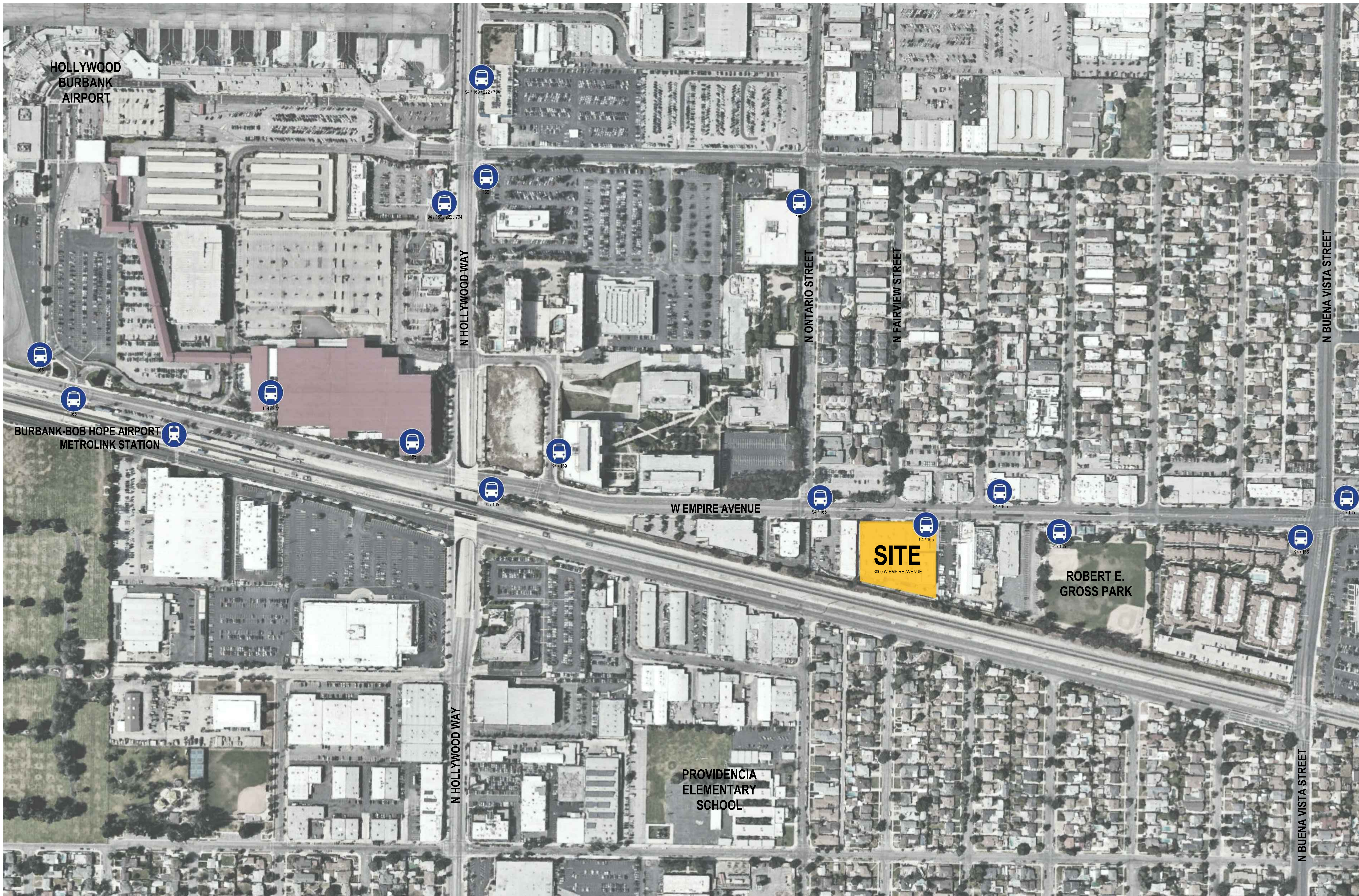
SHEET NAME/NUMBER:

TITLE SHEET

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NOTES/REVISIONS:


PROJECT DATA:

DATE:	12-22-22
ARCHITECT	EM
CHECKED BY	EM
DRAWN BY	EM
PROJECT NO.	-
SCALE	N.T.S.

SHEET NAME/NUMBER:

**SITE MAP**  
**A-0.1**







CONDITIONS PER PUBLIC WORKS TRAFFIC ENGINEERING SECTION  
 No visual obstruction shall be erected or maintained above 3' high or below 10' high in a 5' by 5' visibility cut-off at intersection of street and driveway. [BMC 10-1-1303(C)]  
 Driveway apron shall be constructed per Burbank Standard Plan BS-102.  
 Driveway slope and transition shall be constructed per Burbank Standard Plan BT-406.  
 All driveways and access road leading to parking areas shall be designed to accommodate turn path of Passenger Car (P) design vehicle per the AASHTO Greenbook with minimum 24' outer turning radius and 15' inner turning radius.  
 Parking spaces shall be constructed per BMC 10-1-1401, 10-1-1403, 10-1-1417, 10-1-1606.  
 Parking shall be arranged so that vehicles do not back onto major or secondary highway to exit from the parking area. [BMC 10-1-1403]  
 Parking spaces for residential use shall have a minimum width of 8'-6" and a minimum length of 18' per BMC 10-1-1401.

Access aisle width for standard spaces shall be per Burbank Standard Plan BT-401 as follows:  
 STANDARD SIZE AUTOMOBILE SPACES ADJOINING ACCESS AISLE  
 BASIC STALL WIDTH ACCESS AISLE WIDTH  
 8'-6" 27'-4"  
 8'-8" 26'-8"  
 9'-0" 25'-4"  
 9'-4" 24'-0"

This project is anticipated to increase the volume of pedestrians that will cross Empire Avenue to access the transit stops near Niagara Street/Empire Avenue and Catalina Street/Empire Avenue. Developer shall install either a High Intensity Activated Crosswalk (HAWK) beacon or a conventional traffic control signal at the intersection of Empire Avenue and Catalina Street.  
 The HAWK beacon and traffic control signal are intended to

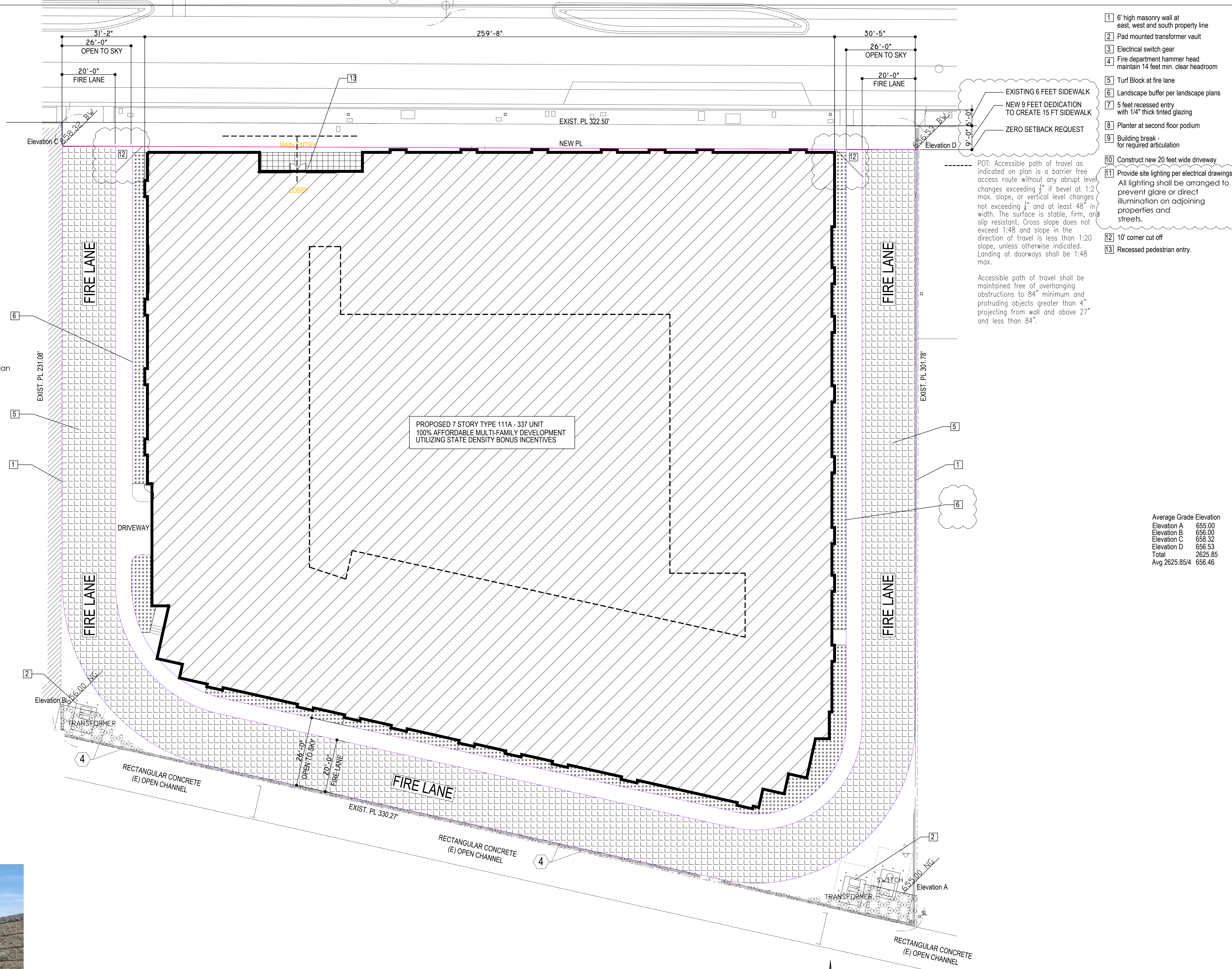
- 1) improve accessibility for the residents of the project to transit stop located on the north side of Empire Avenue and
- 2) reduce the chance of vehicle vs. pedestrian conflicts due to uncontrolled pedestrian crossing at the intersection. Developer shall provide documentation that the signal meets the one of the traffic control signal warrants per the 2014 California Manual on Traffic Control Devices Chapter 4C.

The HAWK beacon or traffic control signal shall meet the following requirements:  
 All improvements shall be subject to the approval of the Public Works Director or his/her designee. Developer shall coordinate with the Public Works Department for the installation of a traffic control signal at the intersection.

Developer shall submit traffic signal installation and striping plans to the Public Works Department, for review and acceptance. These plans shall be submitted with a Public Work Excavation Permit application.  
 High-visibility crosswalks shall be installed at the intersection to provide a clear and accessible pedestrian path across Empire Avenue.  
 The crosswalks shall be installed per City of Burbank Standard Plan BT-409A-1. New ADA ramp(s) shall be installed at the intersection to provide accessible pedestrian path across Empire Avenue.  
 The HAWK beacon shall meet the requirements of Chapter 4F 'Pedestrian Hybrid Beacons' of the 2014 California Manual on Uniform Traffic Control Devices.  
 The HAWK beacon or traffic control signal shall have Caltrans signal and safety lighting standards, 3-inch conduit, a Caltrans 332L controller cabinet, and a Myers USPA service cabinet.

The signing and striping shall be in accordance with 2014 California Manual on Uniform Traffic Control Devices.

Investigate the need to relocate transit stop on the south side of Empire at Empire/Catalina to Empire/Niagara to improve transit accessibility.



- 1 6' high masonry wall at east, west and south property line
- 2 Pad mounted transformer vault
- 3 Electrical switch gear
- 4 Fire department hammer head maintain 14 feet min. clear headroom
- 5 Turf Block at fire lane
- 6 Landscape buffer per landscape plans
- 7 5 feet recessed entry with 1/4" thick tinted glazing
- 8 Planter at second floor podium
- 9 Building break - for required articulation
- 10 Construct new 20 feet wide driveway
- 11 Provide site lighting per electrical drawings  
All lighting shall be arranged to prevent glare or direct illumination on adjoining properties and streets.
- 12 10' corner cut off
- 13 Recessed pedestrian entry.

EXISTING 6 FEET SIDEWALK  
 NEW 9 FEET DEDICATION TO CREATE 15 FT SIDEWALK  
 ZERO SETBACK REQUEST

POT: Accessible path of travel as indicated on plan is a barrier free access route without any abrupt level changes exceeding 1/4" if level at 1:2 max. slope, or vertical level changes not exceeding 1/4" and at least 48" in width. The surface is stable, firm, and slip resistant. Cross slope does not exceed 1:48 and slope in the direction of travel is less than 1:20 slope, unless otherwise indicated. Landing at doorways shall be 1:48 max.

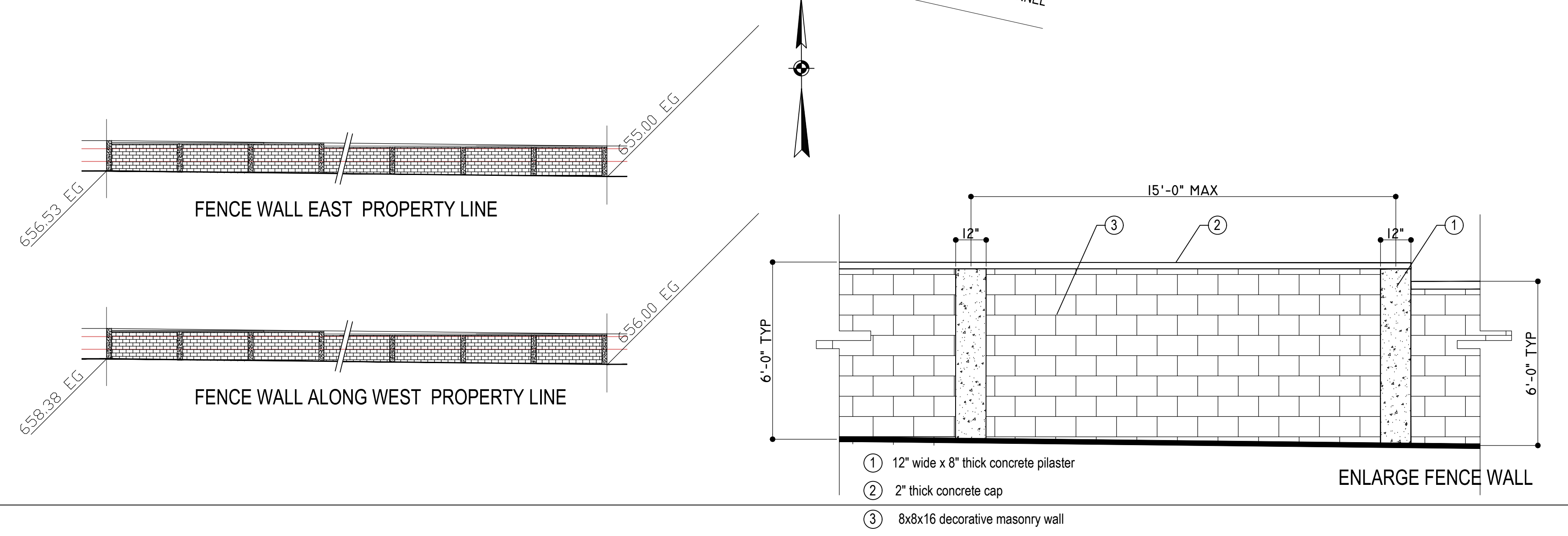
Accessible path of travel shall be maintained free of overhanging obstructions to 84" minimum and protruding objects greater than 4" projecting from wall and above 27" and less than 84".

Average Grade Elevation	
Elevation A	655.00
Elevation B	656.00
Elevation C	658.32
Elevation D	656.53
Total	2625.85
Avg	2625.85/4 656.46



STOCK PICTURE OF FENCE WALL

Utility Poles: There are no Utility Poles on site.  
 10-1-1602: CURB CUTS  
 No vehicular access way shall be located nearer than 30 feet to the ultimate curb lines of an intersecting street, nor be provided with a curb cut of more than 18 feet in residential zones and 30 feet in other zones. The Public Works Director may permit a curb cut of not more than 38 feet in nonresidential zones if the adjacent parking area is provided with an internal circulation pattern requiring two (2)-way vehicular movement in the driveway. Curb cuts on the same lot shall be separated by at least 20 feet of uncut curb. In residential zones, each lot is limited to one curb cut for each 100 feet of street frontage along any one street except that lots with less than 100 feet of street frontage may provide one curb cut. Minor deviations from the foregoing standards may be authorized by the Public Works Director to accommodate the safe ingress and egress of vehicles.  
 Curb cut will be constructed per City of Burbank Standard plans



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LICENSED ARCHITECT  
 EDWIN R. MOHABIR  
 C23674  
 Exp. 02-28-23  
 STATE OF CALIFORNIA

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SCALE: 1/16" = 1'-0"

SHEET NAME/NUMBER:  
**SITE PLAN  
 A-2.0**

























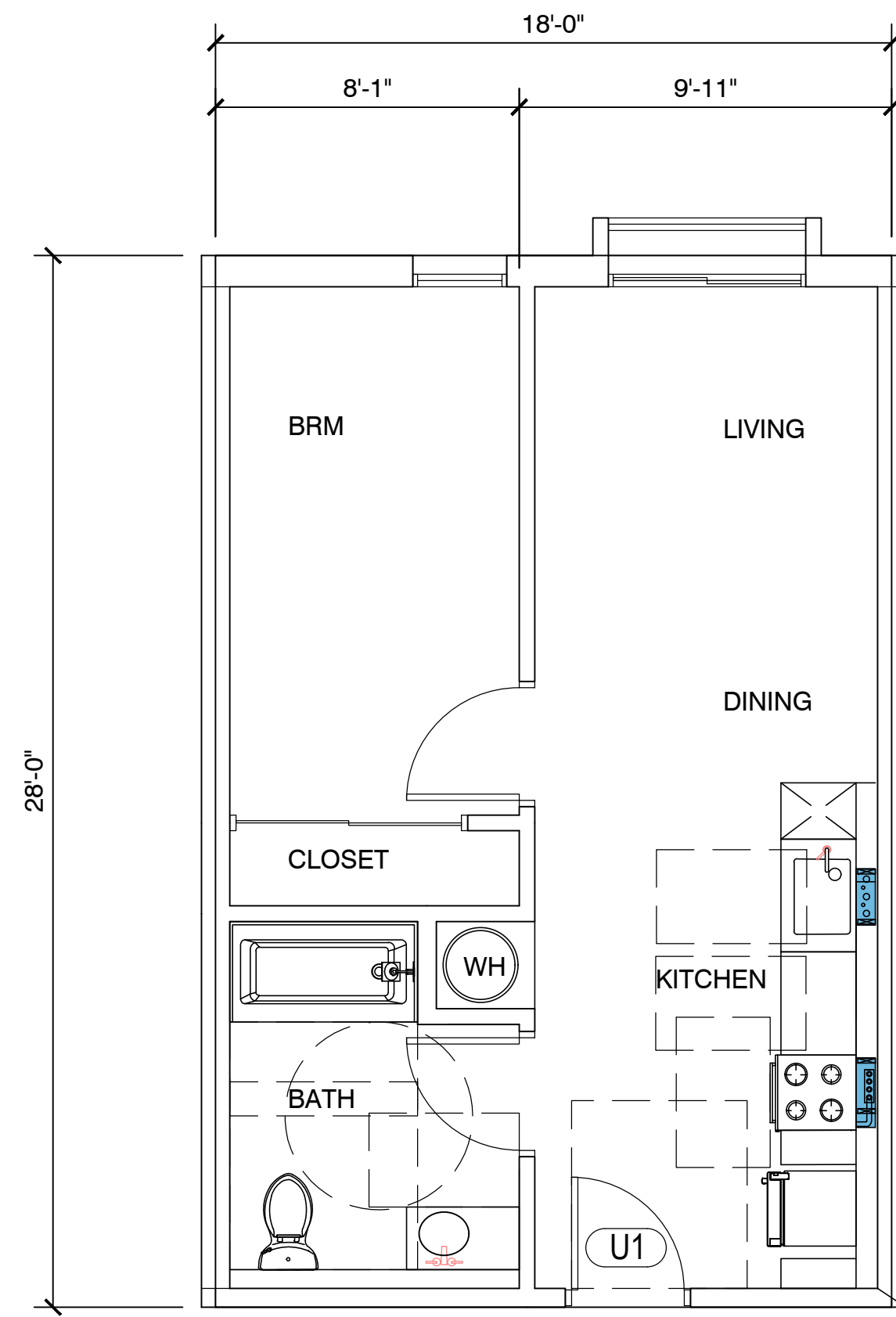




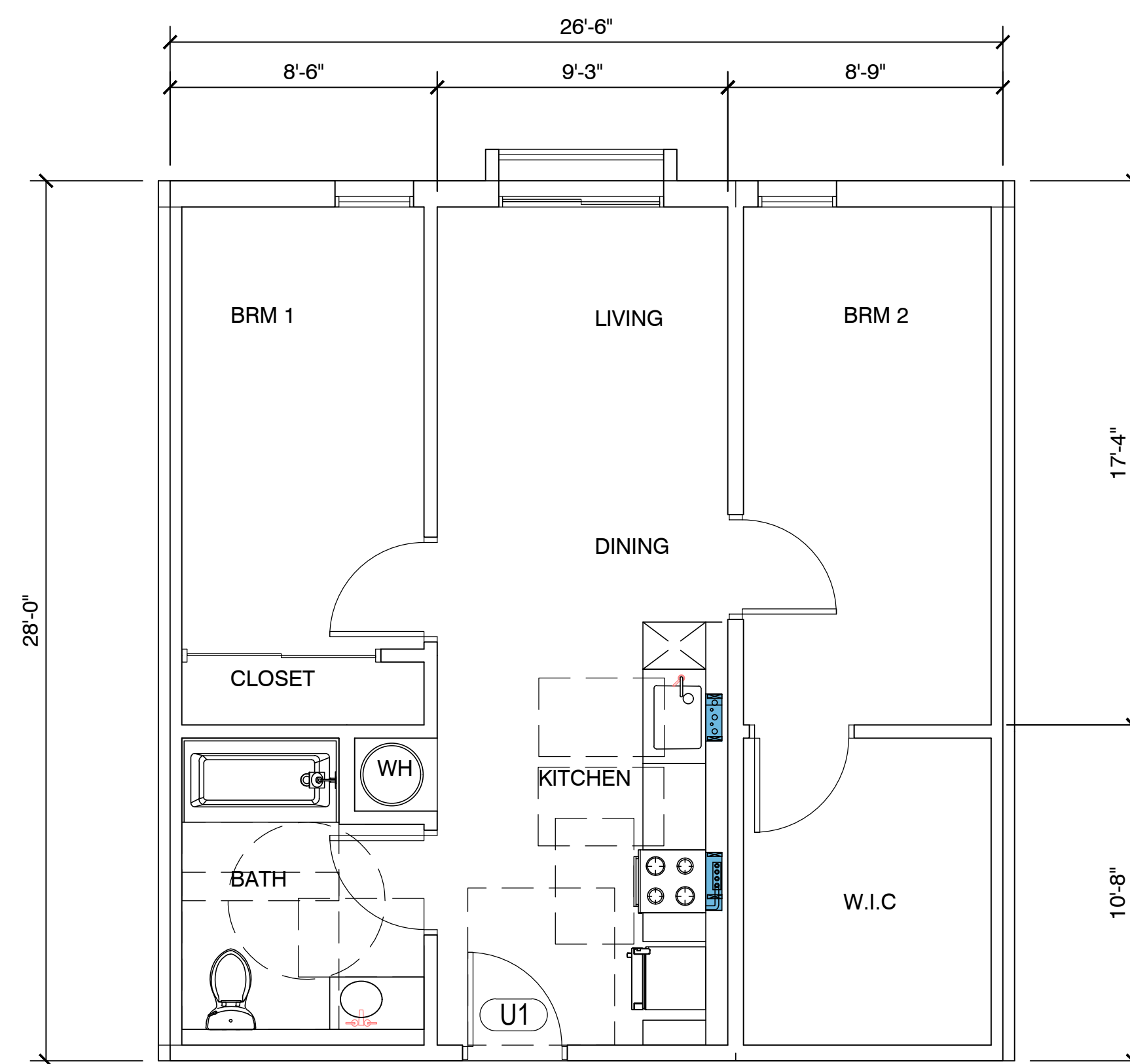




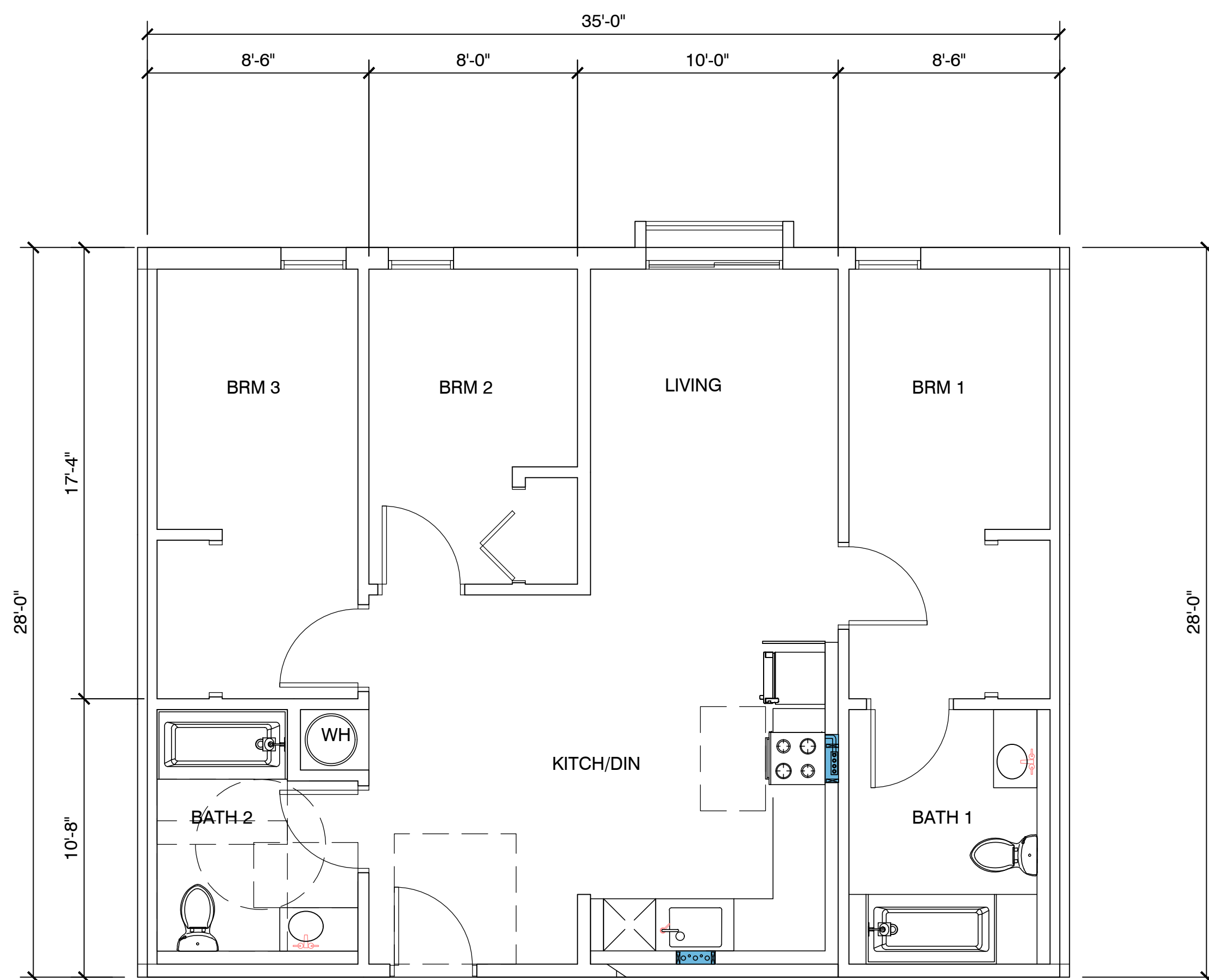




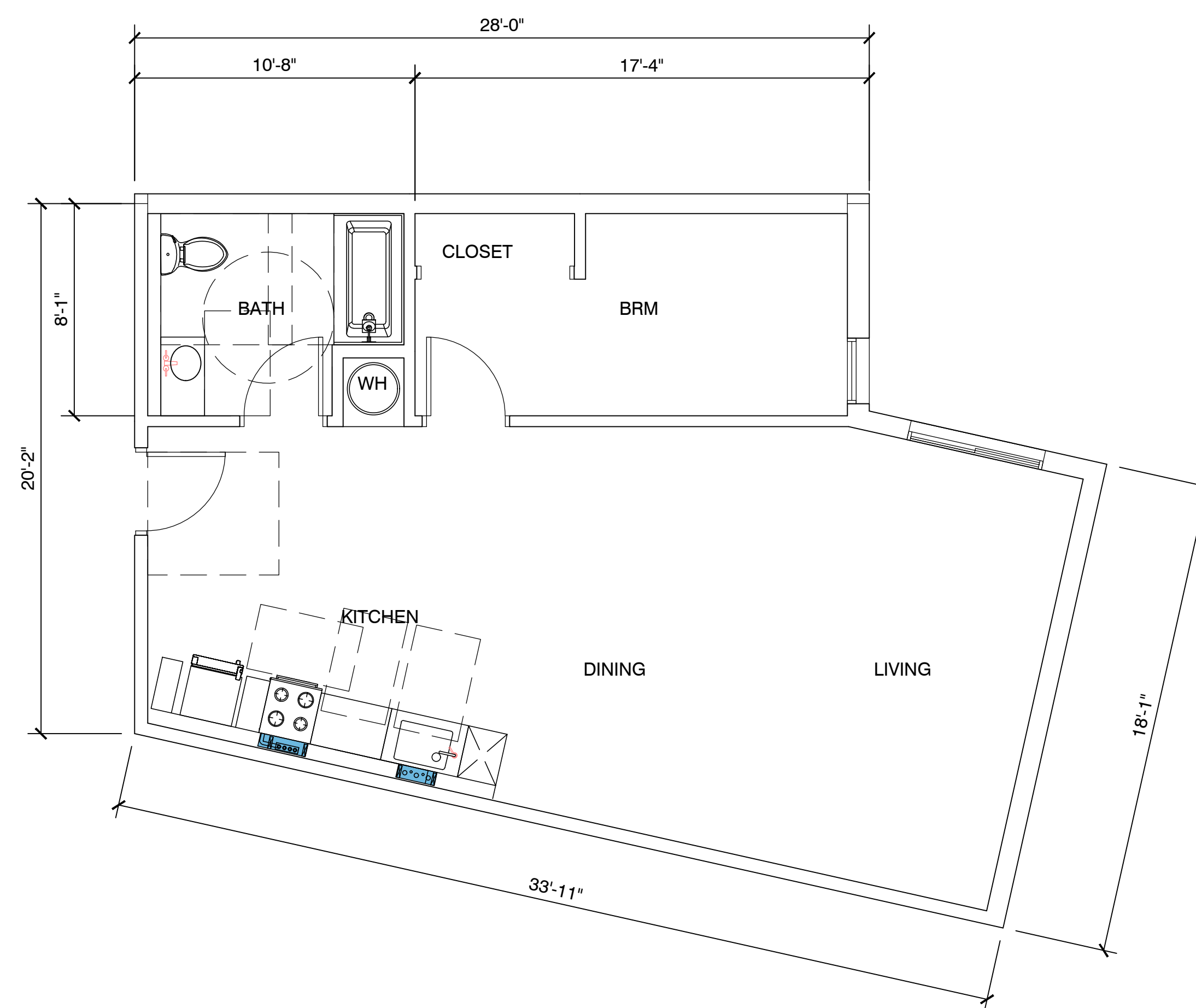
UNIT TYPE A - 1 BRM  
504 S.F



UNIT TYPE B - 2 BRM  
740 S.F



UNIT TYPE C - 3 BRM  
980 S.F



UNIT TYPE D - 1 BRM  
760 S.F

Operable parts shall be operable with one hand and shall not require tight grasping, pinching, or twisting of the wrist. The force required to activate operable parts shall be 5 pounds (22.2 N) maximum.

- 1 Electric Range
- 2 18" ADA complying Dishwasher
- 3 HVAC Soffit - 7' min above finish floor
- 4 Provide mechanical ventilation which furnishes five air changes per hour direct to the outside.
- 5 Provide hardwire smoke detector/CO sensor with battery back up. See floor plan for location.
- 6 Prefab fiber glass tub enclosure walls are 70" high above the drain. Solid Joint at trap-no access panel required.
- 7 All water closet must be low water consumption ULTRA FLUSH.
- 8 removeable base cabinets under sink counter work area
- 9 30" wide work surface
- 10 grab bar backing typical
- 11 lever handles on all fixture typical
- 12 medicine cabinet
- 13 42" high guardrail typ.
- 14 24" wide refrigerator
- 15 LVT flooring throughout
- 16 ADA kitchen sink
- 17 ADA bathroom sink
- 18 provide single shelf and single pole
- 19 cultured stone counter top at all kitchen, laundry room. repositionable counter top not required per exception
- 20 cultured marble bathroom counter top.
- 21 exhaust hood to outside and 3 feet from window openings
- 22 Electric Water heater. Provide water sub meter in unit
- 23 Provide hard wire door bell 48" a.f.f to top of device Provide 2 door viewer one mounted at 43" max a.f.f

- LEGEND:
- vert. wall reinf for future swing up grab bar
  - horiz. reinf for future wall mounted grab bar

- HARDWARE SMOKE DETECTOR/CO SENSOR W/ BATTERY BACKUP

EXHAUST FANS SHALL BE ENERGY STAR COMPLIANT AND CONTROLLED BY HUMIDISTAT. 4.506.1

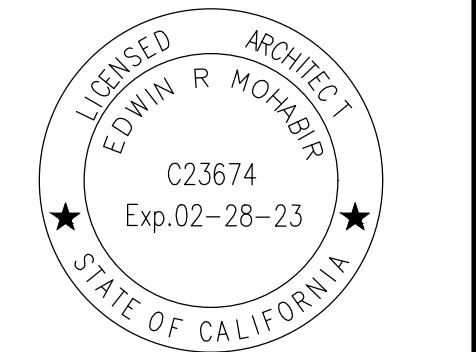
ALL PLUMBING FIXTURES SHALL COMPLY WITH TABLE 4.303.2

EACH APPLIANCE SHALL BE ENERGY STAR COMPLIANT IF APPLICABLE FOR THAT APPLIANCE. 4.210

PROVIDE DUCTED FRESH AIR INTAKE TO HVAC UNIT

PROVIDE RANGE HOOD AT ALL KITCHEN PER TITLE 24

- 30" X 48" CLEAR FLOOR SPACE



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337 AFFORDABLE  
HOUSING DEVELOPMENT

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Empire II**

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DEVELOPER INFORMATION:

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5500 HOLLYWOOD BLVD., 4TH FLR  
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SHEET NAME/NUMBER:

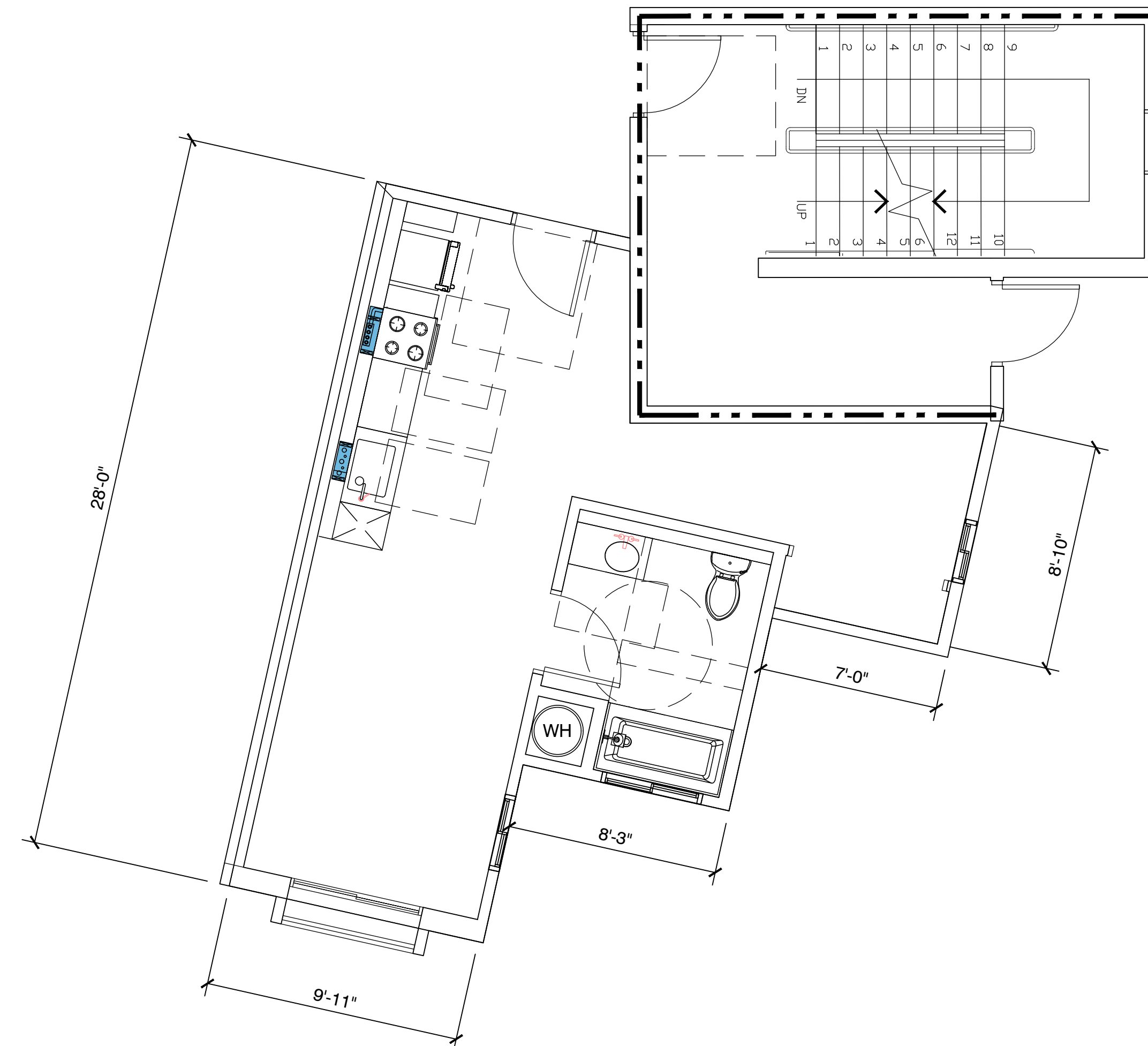
TYPICAL UNIT  
PLANS

A-3.0





UNIT TYPE E - I BRM  
550 S.F



UNIT TYPE F - STUDIO  
440 S.F

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- 15 LVT flooring throughout
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- 17 ADA bathroom sink
- 18 provide single shelf and single pole
- 19 cultured stone counter top at all kitchen, laundry room. repositionable counter top not required per exception
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Provide 2 door viewer one mounted at 43" max a.f.f

- LEGEND:
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  - horiz. reinf for future wall mounted grab bar

SD HARDWIRE SMOKE DETECTOR/CO SENSOR W/ BATTERY BACKUP

EXHAUST FANS SHALL BE ENERGY STAR COMPLIANT AND CONTROLLED BY HUMIDISTAT. 4.506.1

ALL PLUMBING FIXTURES SHALL COMPLY WITH TABLE 4.303.2

EACH APPLIANCE SHALL BE ENERGY STAR COMPLIANT IF APPLICABLE FOR THAT APPLIANCE. 4.210

PROVIDE DUCTED FRESH AIR INTAKE TO HVAC UNIT

PROVIDE RANGE HOOD AT ALL KITCHEN PER TITLE 24

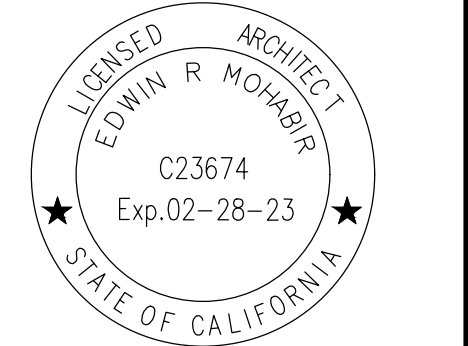
30" X 48" CLEAR FLOOR SPACE

EDWIN MOHABIR

**EM**

ARCHITECT, INC.

25206 BISHOP CT.  
STEVENS RANCH, CA 91381  
EDWINMOHABIR@GMAIL.COM  
TEL: 323-459-8809



PROJECT ADDRESS:  
337 AFFORDABLE HOUSING DEVELOPMENT

**Residency at Empire II**

3000 W. EMPIRE AVE  
BURBANK, CA

DEVELOPER INFORMATION:

**ABS Properties, Inc.**  
5500 HOLLYWOOD BLVD., 4TH FLR  
LOS ANGELES, CA 90028  
(213)268-2723

NOTES/REVISIONS:

PROJECT DATA:

DATE: 12-28-22  
ARCHITECT: EM  
CHECKED BY: EM  
DRAWN BY: EM  
PROJECT NO.:

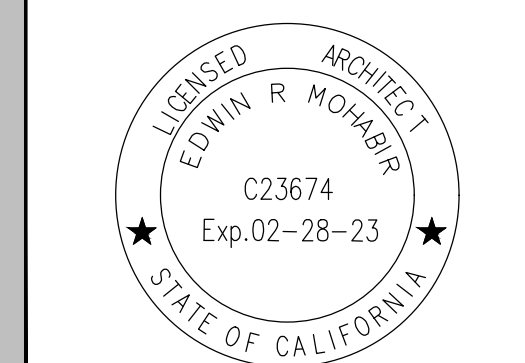
SCALE: 1/4" = 1'-0"

SHEET NAME/NUMBER:

TYPICAL UNIT PLANS

A-3.1





PROJECT ADDRESS:  
337 AFFORDABLE  
HOUSING DEVELOPMENT

### Residency at Empire II

3000 W. EMPIRE AVE  
BURBANK, CA

DEVELOPER INFORMATION:

**ABS PROPERTIES, INC.**  
5500 HOLLYWOOD BLVD., 4TH FLR  
LOS ANGELES, CA 90028  
(213)269-2723

NOTES/REVISIONS:


PROJECT DATA:

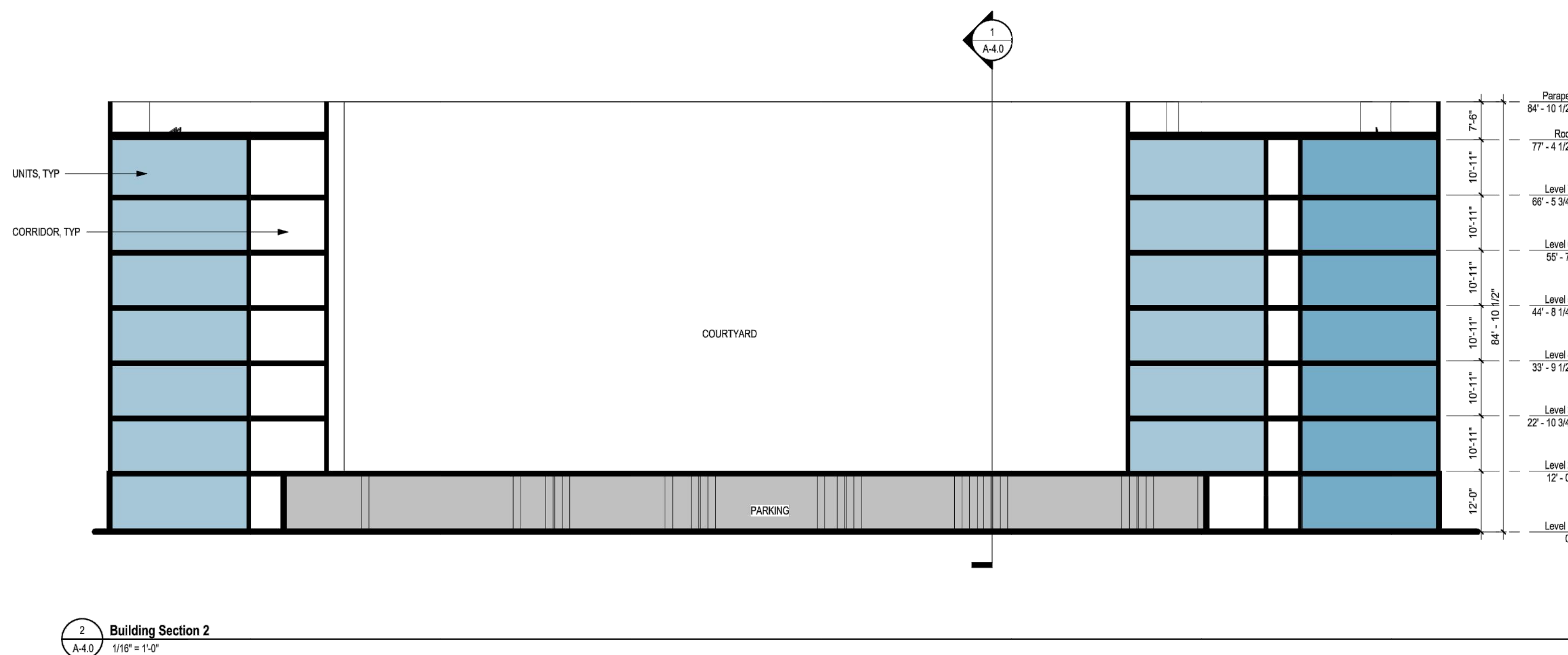
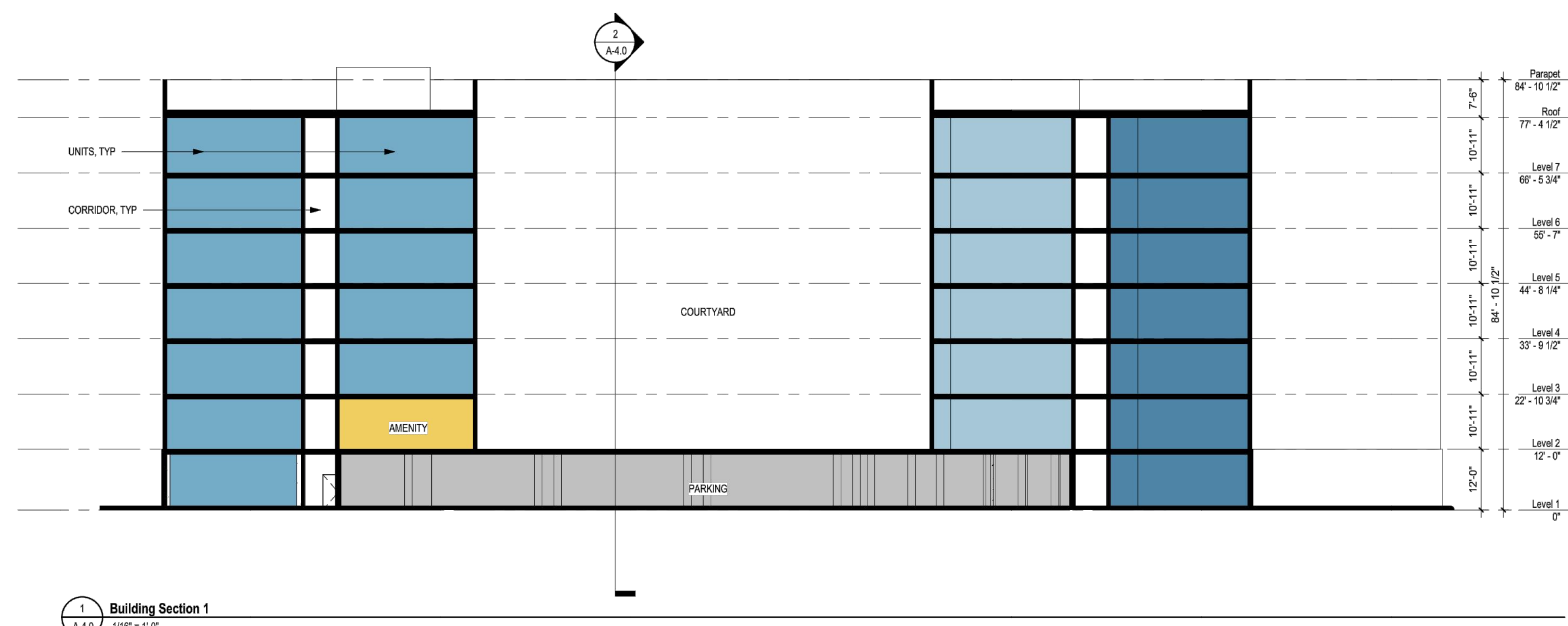
DATE:	12-22-22
ARCHITECT	EM
CHECKED BY	EM
DRAWN BY	EM
PROJECT NO.	-

SCALE 1/16" = 1'-0"

SHEET NAME/NUMBER:

### BUILDING SECTIONS

## A-4.0







Elevation Keynotes	
Mark	Description
1	BRICK MASONRY - RED/BROWN
2	HORIZONTAL METAL PANEL - DARK GRAY
3	STUCCO OR FIBER CEMENT PANELS - OFF WHITE/ TWO TONES
4	WOOD-LOOK FIBER CEMENT SIDING
5	METAL MESH BALCONY RAILINGS
6	METAL TRELIS WITH WOOD MEMBERS
7	BOARD-FORMED CONCRETE
8	PREFINISHED ALUMINUM MECHANICAL SCREEN



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LICENSED ARCHITECT  
EDWIN R. MOHABIR  
C23674  
Exp. 02-28-23  
STATE OF CALIFORNIA

PROJECT ADDRESS:  
337 AFFORDABLE HOUSING DEVELOPMENT

**Residency at Empire II**

3000 W. EMPIRE AVE  
BURBANK, CA

DEVELOPER INFORMATION:  
**ABS PROPERTIES, INC.**  
5500 HOLLYWOOD BLVD., 4TH FLR  
LOS ANGELES, CA 90028  
(213)268-2723

NOTES/REVISIONS:


PROJECT DATA:

DATE:	12-22-22
ARCHITECT	EM
CHECKED BY	EM
DRAWN BY	EM
PROJECT NO.	-
SCALE	1/16" = 1'-0"

SHEET NAME/NUMBER:  
**EXTERIOR ELEVATIONS**  
**A-5.0**